

Town of Oriental

Boating Infrastructure Grant Program Application
FY 2012



"The Sailing Capital of North Carolina"



Oriental's Raccoon Creek Harbor view from existing 50' municipal transient dock in height of shrimp season.

A. Need

Oriental, the acclaimed "Sailing Capital of North Carolina" needs transient dockage to accommodate recreational boaters who wish to experience its unique charm as a destination, or facilitate mariners just passing by this remarkable waterway village. Located on the north shore of the Neuse River on the Intracoastal Waterway (ICW) at the Mile 182 waypoint, Oriental offers a treasure chest full of resources to all, begging for the taking. The town, populated by only 900 permanent residents, has in excess of 2,700 boating slips! However, most dockage is private and is associated directly with the owners of shore property, or are slips leased for long term to provide a home port for owners living further inland.

The town's economy was once centered upon the commercial harvest from the 4-mile-wide Neuse and the Pamlico Sound 15 miles to the east. It now relies more heavily upon the destination tourist and transient cruising boaters for its livelihood. To many a boater's dismay, upon arrival for a short stay at this renowned boater's paradise, offshore anchorage with dinghy-in transportation is the only option to come ashore to enjoy our offerings at minimal expense.

Oriental does not have a major highway passing by it, as it is located at the end of NC HWY 55 making our town only a "true destination". However, it is fortuitously located only one mile from the highly traveled ICW approximately half way between the state's north & south borders. Furthermore, with Oriental's location on the Neuse at the mouth of the Pamlico Sound accessing Ocracoke and the Outer Banks, it puts us at the intersection of much marine travel on course to the four points of the compass.

Raccoon Creek Harbor, the site of the proposed dock, forms the core of Oriental's "attraction center" displaying the quaint fishing village's charm. Countless prints and paintings, with subjects or inspirations captured at this site adorn walls, albums and memories of visitors and travelers international in scope. Many of the town's now permanent residents were just "passers-by" at some point in time, and were captivated by the area's allure to such a point where they choose to return here to live by will and design.

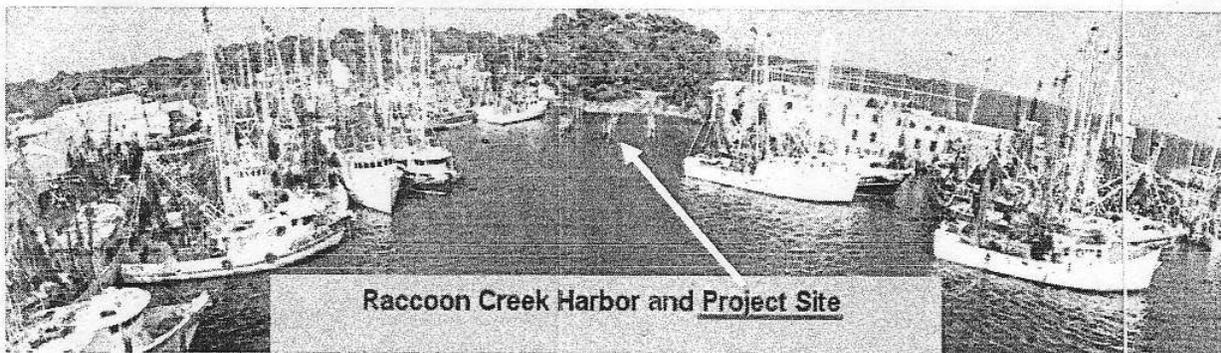
The current August 2011 issue of Soundings Magazine features Oriental as one of the six "perfect picks" along the Intracoastal Waterway from Maine to Florida. An Oriental scene is depicted as the articles' "feature" photo of the six locations on page 37, and the harbor location for our proposed transient dockage appears in the article specific to Oriental in the photo on page 42.

The town currently offers a transient **no-fee** 50 foot dockage at the northern end of the harbor (Figure 2), which can only accommodate three 26' or larger vessels at a time. This facility is town maintained and regulated by a 48-hour-maximum occupancy per month policy for any one vessel. It is in constant use, and at many times one may witness multiple vessels voluntarily "rafting" in arguably precarious fashions, vying for the precious access to the village's assets.

A table of dockage and facilities within 2 miles of the proposed site is provided below.

Facility Name	Total slips	Transient	Fees	Pump-out	
Oriental Municipal Town Dock	3	3	No	None	Town plans to use future proposed
Pecan Grove Marina	200	0	Yes	Owners	Cart - for slip owners
Clancy's Marina	15	0	Yes	Yes	Restricted to 45' bridge to 3' of water
Oriental Harbor Marina	110	15 *	Yes	Owners	* \$275 monthly 30 foot
Oriental Marina and Hotel	12	12	Yes	None	\$1.90/ft per day
Whittaker Creek Yacht Harbor	180	25	Yes	Owners	\$2/day/ft No liveaboards / \$15 Pump out
Whittaker Pointe	53	5	Yes	Yes	\$1.25/day/ft Pump out
THIS PROPOSED DOCK	6	6	No	Planned	Planned in second phase of development

Oriental has observed how the BIG-Program has been instrumental in the development of facilities in the neighboring counties of Carteret, Craven, and Beaufort, which has contributed to their area accessibility and economic growth. We would like to affect a similar success story to our often overlooked recreational-resource-rich county of Pamlico.



B. Objectives

Provide safe, free dockage for up to six 26' or longer recreational vessels convenient to the center of Oriental's harbor district to afford transient boaters an opportunity to experience our village's maritime culture while fostering the town's economic development. The project would within 6 months of award transform the site's "neglected look" of remnants of dilapidated dysfunctional piers to a purposefully safe transient vessel dockage basin of sufficient depth within walking distance to village's amenities.

To produce a professionally architected set of plans for purposeful guidance to coordinate the next steps taken in the providing of additional facilities offered to our transient recreational boaters beyond this first essential component of a basin and dock. The plans, to be complete by next grant cycle, will address and provide quantifiable specifics for a comprehensive plan to efficiently provide facilities amenities while leveraging our investments and aiding in recruitment of additional buy-in support.

C. Expected Results and Benefits

This project will be the cornerstone of a comprehensive transient boating facility to be expanded in future stages to include a mooring field, a pump-out station, restrooms, showers, laundry, utilities, local information center and internet access. All these needs were identified during our town's Vision Statement development process, and the basin and dock completion is a standalone action step fulfilling this guiding document directions. Site acquisition for the additional facilities' and their design is an ongoing process, in which we will be assisted by professional consultants including Rivers & Associates.

An analysis of the expected economic impact of the requested 6 new transient slips is based on the On-Line Boating Economic Impact Calculator developed by the Recreation Marine Research Center at Michigan State University. The Impact Calculator estimates trip spending per transient boat applied to economic ratios and multipliers that reflect the local economy. For the purposes of this analysis, each of the requested 6 slips was estimated to host 200 "boat days", half for power boats and half sailboats, resulting in the following economic impact estimates for 1,200 more Oriental "transient boat days" annually. Remembering dockage is free.

1. \$15,500 in additional sales
2. \$5,700 in additional labor income
3. \$7,600 value added to existing facilities and services

The demolition required before the dredging operation will rectify the long "neglected waterfront" appearance at the entrance to our town harbor. The building of a modern dock here will "roll out the red carpet welcome mat" to boaters by providing a safe, convenient, affordable (free) temporary dockage at the most visited location within the town.

One of the largest artificial fishing reefs in inland waters (some 65 acres) is being developed barely 2 miles east of the proposed dock site. This attraction and our project will complement each other to provide an even greater allure to the renowned sport fishing opportunities afforded by a visit and stay at Oriental.

The economic impact to local business such as our restaurants, gift shops, provisions vendors, farmer's market, museum and extensive marine related resources like full service repairs and sail makers, although subjective to quantify, will be self evidently increased.

There will be no initial user fees as maintenance will be provided by the Town of Oriental which collects a 3% occupancy room tax from land based room rentals. These restricted funds are limited to tourism related activities including development and maintenance of Oriental's waterfront access facilities.

D. Approach

Project management will be overseen by Robert Maxbauer, Town Manager who is a NC General Contractor License Qualifier for all categories of construction (NC Unclassified, Unlimited) which include all marine, utilities and infrastructure aspects of this project.

Even though a 14' deep channel, maintained by the Army Corps of Engineers, affords access to the proposed dock's end, dredging will be required to provide adequate access along the dock's length.

Our plans include the dredging of 1200 cubic yards to provide a minimum basin depth of 8 feet extending 30 feet on either side of the dock for maneuvering and dockage. (Figures 3 & 4) attached depict the dredging specifics as well as does this reference to the link supplied here:
http://www.saw.usace.army.mil/nav/SIDE_CHANNELS/Smiths_Creek/SmithsCk_Oriental.pdf

The dredging company owned & operated by Eric Pake of Beaufort, NC who has specific experience in multiple dredging projects within the Raccoon Creek harbor (including that immediately adjacent to our project) has been consulted, made site inspections and soundings and has contributed directly to our statements. Pake's company will be dredging for this proposed project's adjacent riparian rights holder this fall with permits already secured on the side of his docks opposite ours.

Dredging and construction will be bid let, with construction inspections and site supervision provided by the Town Manager as indicated above. Raccoon Creek is not considered to have special considerations beyond obvious in dredging restrictions as to be in compliance with NEPA, ESA as well as NHPA.

The dock is to be six foot wide extending approximately 120' perpendicular to shore to access the harbor channel. Dockage will be in a basin formed along either side providing 200' of linear dock space so as to provide a minimum of six qualifying transient vessels slips (more with agreed rafting) and providing the versatility to combinations of more lengthy vessels. In addition there will be 200 sq ft of "floating surface" in water too shallow to be used by deep draft for ease of boarding by craft of anchored vessels.

The dock will terminate on the land right-of-way controlled by the Town of Oriental and will connect directly to the city's streets and sidewalks. There will be parallel vehicular parking on both sides of right-of way providing a minimum of one and a half parking places available per slip provided. Construction is to include accessible grading and proper landscape aesthetics to compliment entrance to the dock facility from land. Dockside potable water availability to be provided by the town which operates the municipal water system. Solid waste (trash & recyclables) containers will be esthetically concealed, but convenient at dock's terminus provided and serviced by the town.

Rivers & Associates of Greenville has prime consideration for future design and engineering. This firm is noted because of its diversity and track record of experience of working with other coastal communities and entities in providing visionary facilities and infrastructure development.

This is a maritime community with exhaustive experience resources right here from which to draw. One does not have to travel far for consultation, as the immediate area is populated by marine contractors and operators as well as seasoned "old salts" from many travels. The monies spent here will go for the greater part directly into the local economy and a "pride in workmanship" will most certainly go into each facet as this will be "signature work" locally and give "bragging rights" in participation.

E. Location

The GPS coordinates are: 35° 01' 26" N , 76° 41' 44.5" W

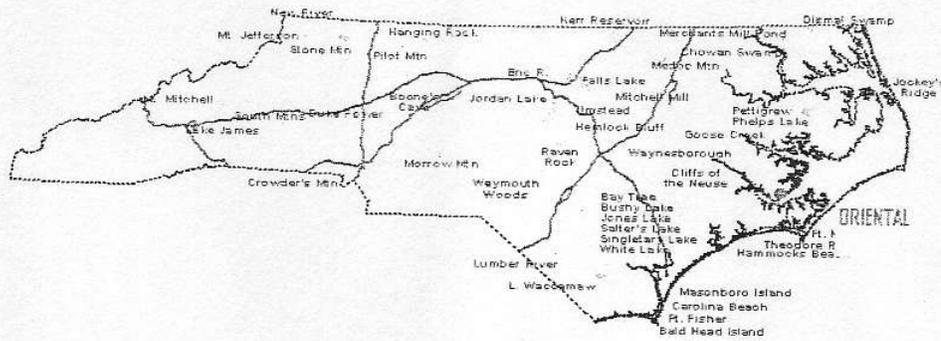


Figure 1. Location of Oriental in North Carolina on ICW at Mile 182 waypoint.

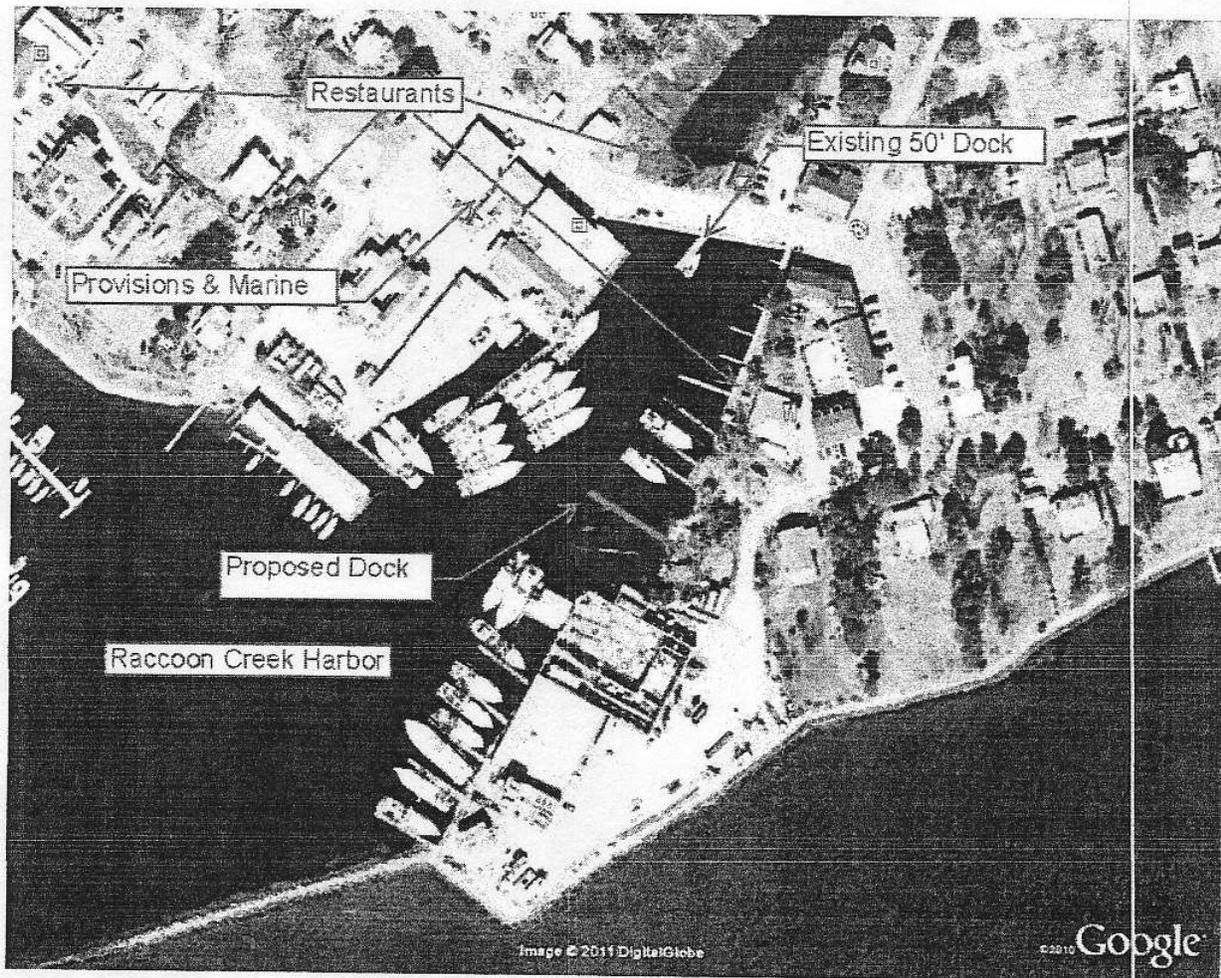


Figure 2. Proposed Oriental Raccoon Creek Harbor Transient Dock

F. Estimated Cost

Budgeting Summary			
		Grant	Match
Project planning, engineering and permits	7,500	5,625	1,875
Site Demolition (Removing debris)	3,000	2,250	750
Dredging (1200 yds @ \$18/yd)	21,600	5,994	15,606
Dock construction (780sqft 130x6 & 200sqft floating)	26,500	19,875	6,625
Dock terminus walkway & landscaping	4,000	3,000	1,000
Potable water service tap & install	3,500	2,625	875
Solid waste containers & concealment	500	375	125
Gross project cost before proration	66,600		
Less Non-eligible use proration 10%	\$59,940	\$39,744	\$26,856

G. Timeline/Milestones

Project dates range from August 2011 through August 2012

	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	June	July	Aug	Sept
BIG-P Application Submission	x													
Grant Awarded							x							
Permitting		x	x	x	x	x	x	x	x					
Contract & Funding									x					
Bidding Process									x	x				
Awarding Construction Contract										x				
Site Construction										x	x	x	x	x
Completion Reporting														x

H. Other Information

Federal Tax ID # is 56-1092769
Fiscal year runs July 1 through June 30
DUNS # 960457091
Town Website www.TownofOriental.com

Contact:

Robert (Bob) Maxbauer
Town Manager, Town of Oriental
PO Box 472
507 Church Street
Oriental, NC 28571-0472
Manager@TownofOriental.com
252-249-0555 Town Hall
252-675-7238 cell

Supporting Documents Follow as Attachments



**CAMA / DREDGE & FILL
GENERAL PERMIT**

New Modification Complete Reissue Partial Reissue

Previous permit # _____
Date previous permit issued _____

As authorized by the State of North Carolina, Department of Environment and Natural Resources and the Coastal Resources Commission in an area of environmental concern pursuant to 15A NCAC

7H-1200

² Rules attached.

Applicant Name Town of Oriental
Address PO Box 472
City Oriental State NC ZIP 28571
Phone # 252-249-0555 Fax # _____
Authorized Agent B. Prescott Marine Const.
Affected FEW APTA YES PTS
AEC(s): OEA HMF IH UBA N/A
 PWS FC
ORW: yes no PNA yes no Crit.Hab. yes no

Project Location: County Pamlico
Street Address/ State Road/ Lot #(s) end of South Avenue
Subdivision _____
City Oriental ZIP 28571
Phone # () _____ River Basin Neuse
Adj. Wtr. Body Raccoon Creek (nat/man/unkn)
Closest Maj. Wtr. Body Neuse River

Type of Project/ Activity proposed pier and platform (Scale: 1" = 40')

Pier (dock) length 6x100'
Platform(s) 20x20'
Finger pier(s) _____
Groin length _____ number _____
Bulkhead/ Riprap length _____ avg distance offshore _____ max distance offshore _____
Basin, channel _____ cubic yards _____
Boat ramp _____
Boathouse/ Boatlift _____
Beach Bulldozing _____
Other (0 slips)
Shoreline Length 82'
SAV: not sure yes no
Sandbags: not sure yes no
Moratorium: n/a yes no
Photos: yes no
Waiver Attached: yes no

A building permit may be required by: Pamlico County See note on back regarding River Basin rules

Notes/ Special Conditions No boat slips authorized. Not to encroach into the Channel Setback (as shown). Not to interfere w/ adjacent riparian property areas; setback at least 15'.

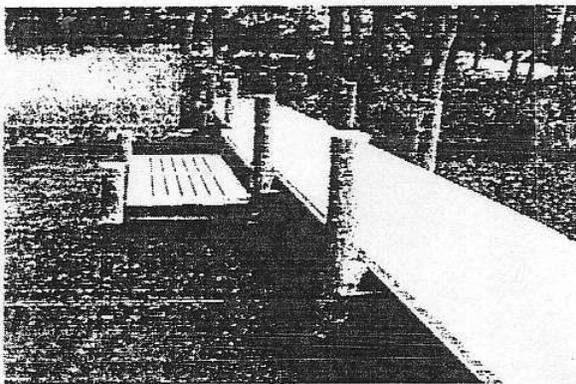
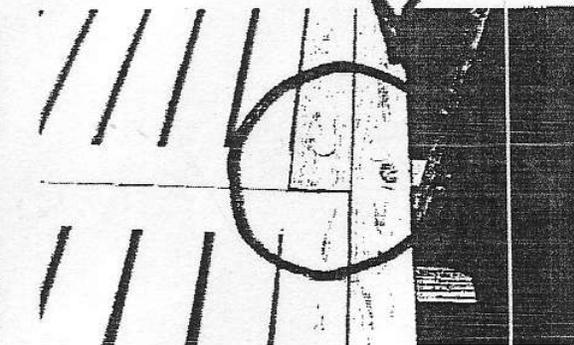
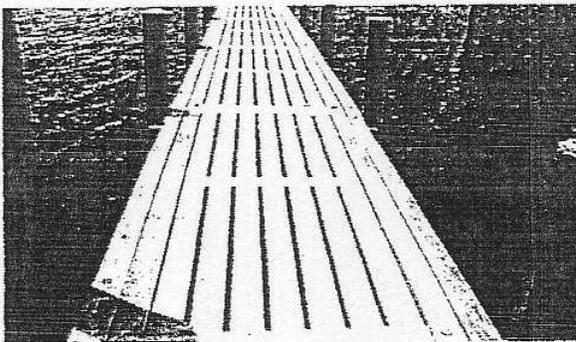
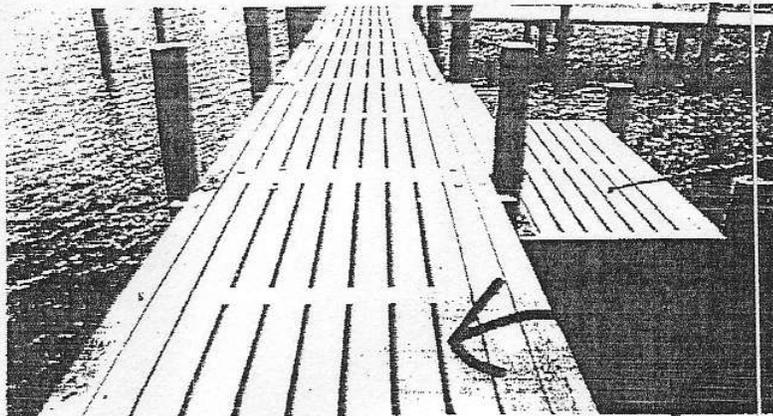
Agent or Applicant Printed Name Brandi Robertson
Brandi P. Robertson
Signature Brandi P. Robertson ** Please read compliance statement on back of permit **
Application Fee (\$) \$200 Check # 2474

Permit Officer's Signature Brad Cornell
Brad Cornell
Issuing Date 6/28/11 Expiration Date 4/30/11
Local Planning Jurisdiction Pamlico County Rover File Name _____

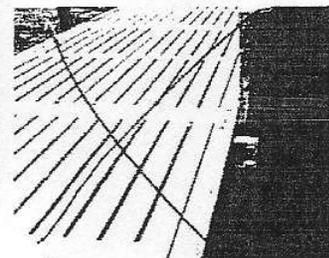
Initial CAMA Approved General Permit for site development . . . Major permit will be required for dredging and extended length equal to adjacent existing docks.

Example of 6' Wide Concrete Dock

Concrete Hog-Slats utilized as decking for piers measure 4'x10'. The slab is then trimmed with two 6"x4" timbers, adding another foot to each side of the decking for a total of 6'.



Above: Example of a Stepdown



To the Left: Examples of the double-slat decking, two 4'x10' slats, trimmed with 6"x4" timbers for a total of 9'.

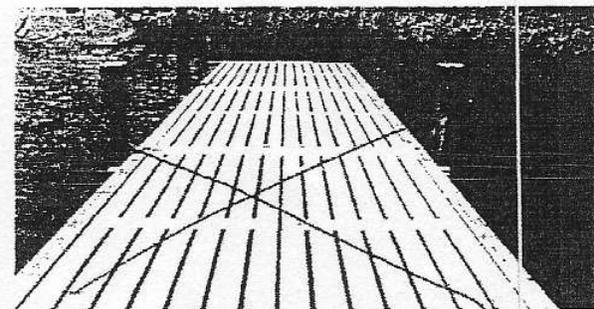


Figure 5 - Proposed 6 foot wide dock to be of concrete surface construction with marine approved anti-corrosion internal reinforcing rebar.

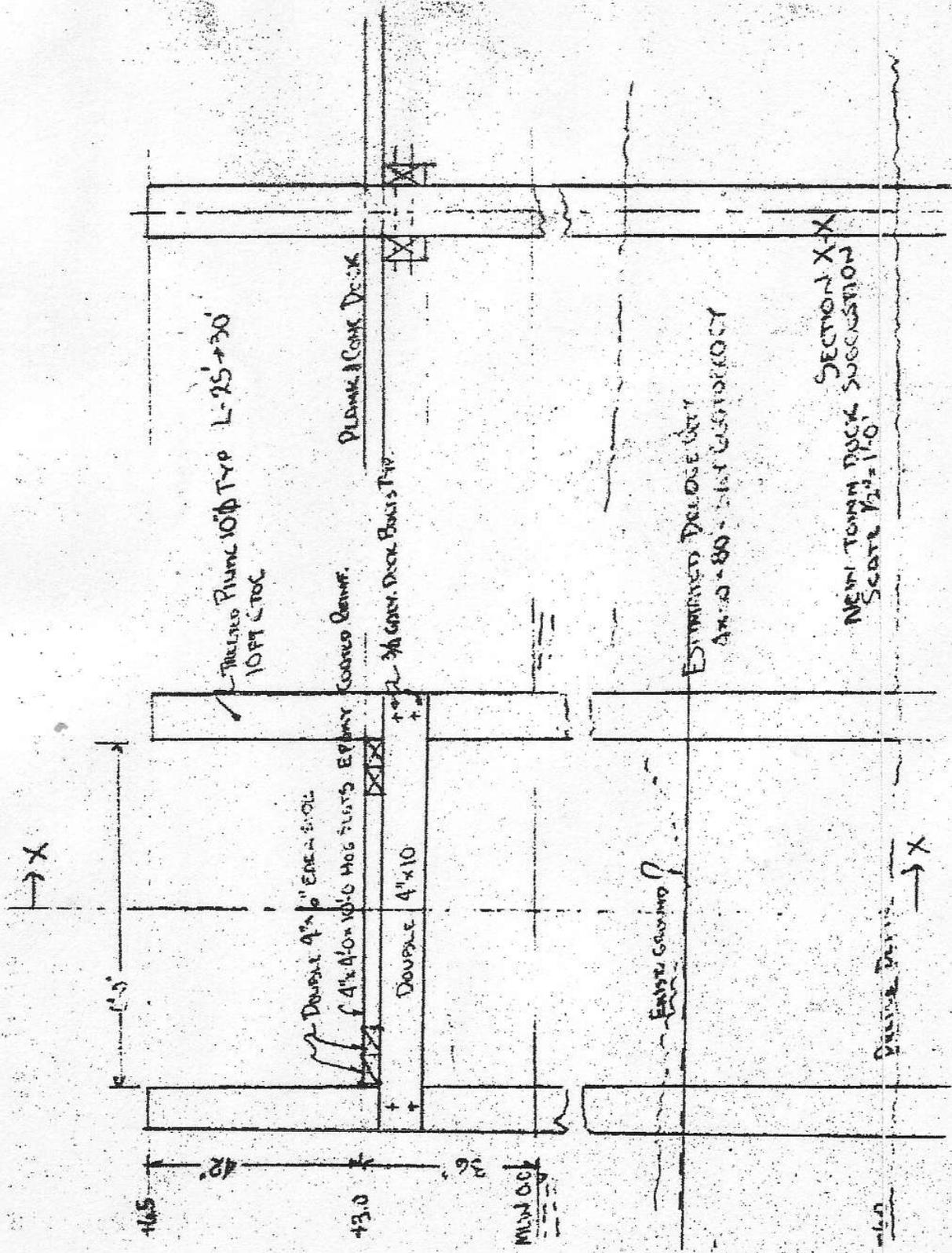


Figure 6 – Oriental Dock construction elevation.

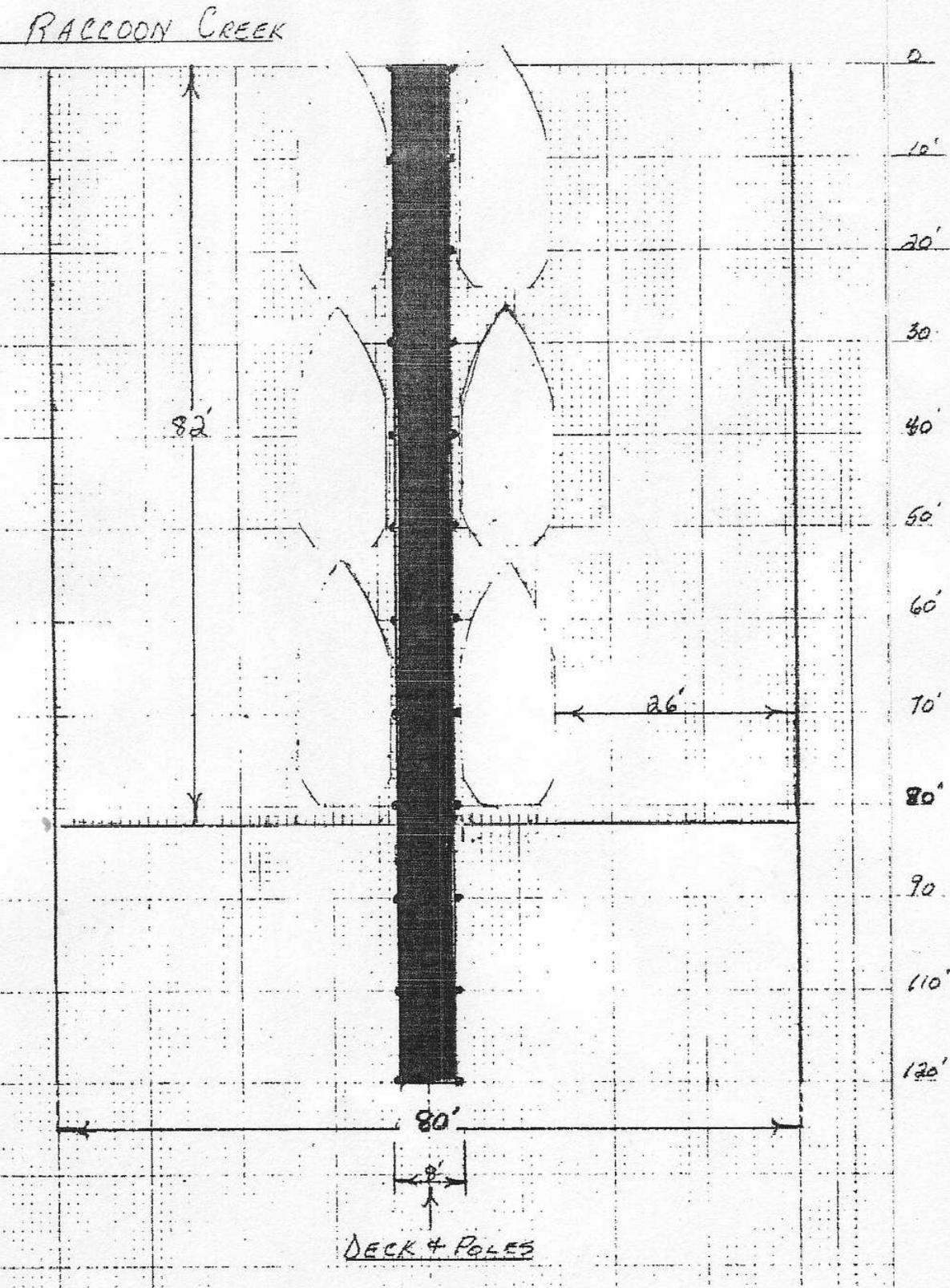


Figure 7 – Oriental Proposed Dock sketch depicting slip computations

**RESOLUTION OF SUPPORT
TO PURSUE A BOATING INFRASTRUCTURE GRANT**

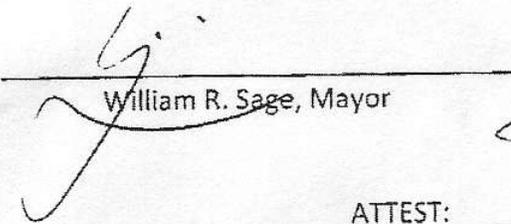
WHEREAS, the Town of Oriental Board of Commissioners has approved funds for the construction of a transient boating dock at the terminus of South Avenue on Raccoon Creek; and

WHEREAS, the Town of Oriental's town manager has developed a site plan including a six foot wide dock extending approximately 100' from the shoreline to a point in line with docks on adjacent properties; and

WHEREAS, dredging of approximately 1,200 cubic yards will be required to make the dock accessible to the majority of recreational cruising boats of 26' or longer.

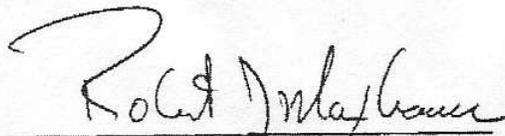
NOW, THEREFORE, BE IT RESOLVED, that the Town of Oriental Board of Commissioners direct the town manager to pursue a Boating Infrastructure Grant to assist with funding the docking facility at the South Avenue terminus on Raccoon Creek limiting the Town's matching funds and dredging expenditures to such amount as already committed to the project by unanimous vote during their May 3, 2011 regular meeting.

ADOPTED THIS THE 2ND DAY OF AUGUST, 2011.



William R. Sage, Mayor

ATTEST:



Robert J. Maxbauer, Town Manager

Figure 8 - Town's resolution of commitment for project grant fund matching and dredging portions not eligible or covered by grant.